



## TECHNO-GRAM 001-2025



**SUBJECT:** Sidewalk Ramps

**PURPOSE:** To revise the Prince George's County Standard for Sidewalk Ramps and to ensure conformance with the Americans with Disabilities Act (ADA) and the Maryland State Highway Administration (MSHA) Standards

**SCOPE:** This techno-gram establishes revised standards for sidewalk ramps.

Effective immediately, permit applicants shall submit permit plans that adhere to the attached Department of Public Works and Transportation (DPW&T) *Specifications and Standards for Sidewalk Ramps*.

APPROVED BY:

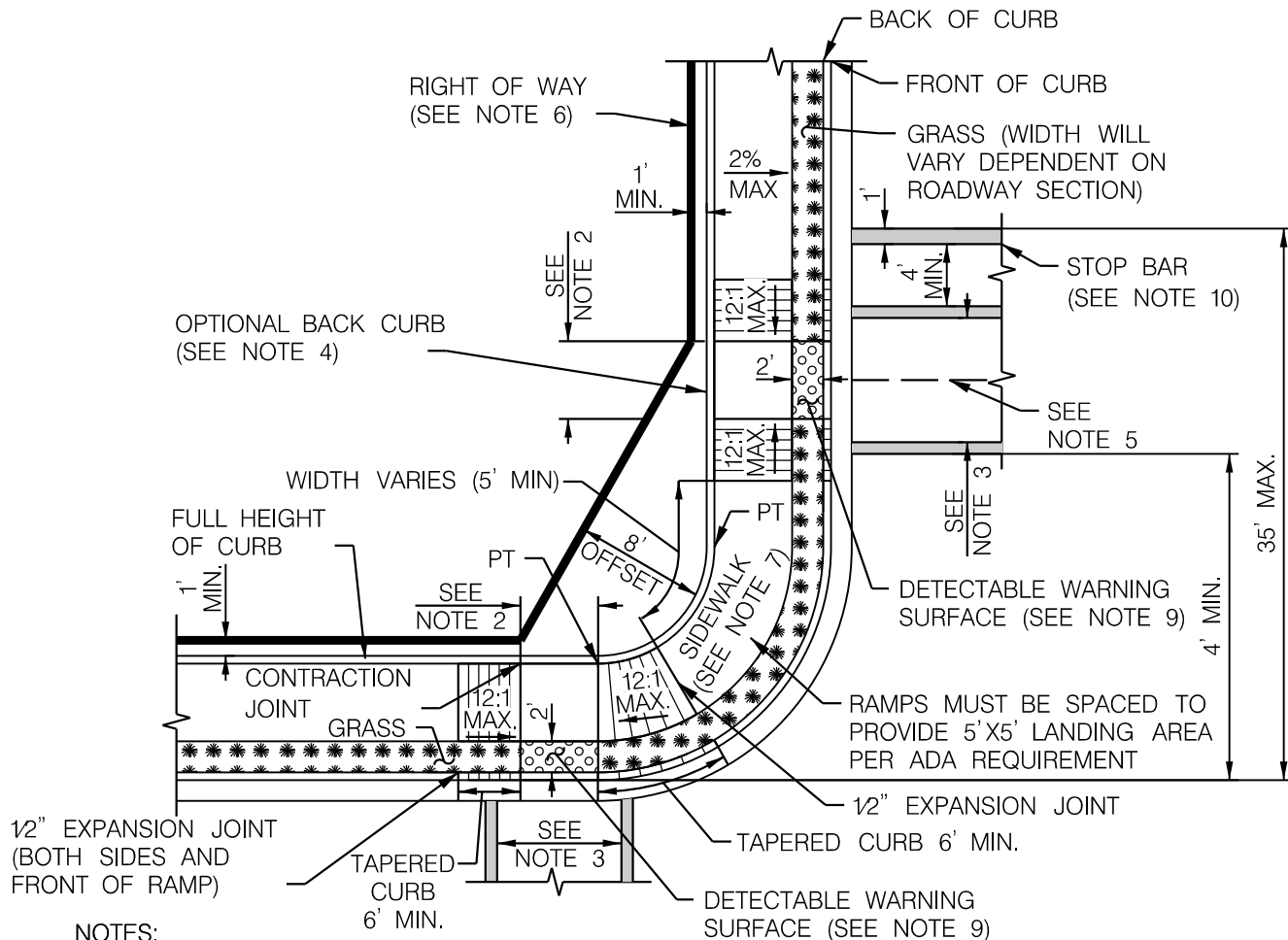
A handwritten signature in cursive script that reads "LaMont A. Hinton".

LaMont A. Hinton, Deputy Director

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**Dawit Abraham, P.E.**  
**Director**

**January 17, 2025**



**NOTES:**

1. SEE PRINCE GEORGE'S COUNTY, SPECIFICATIONS AND STANDARDS FOR ROADWAYS AND BRIDGES, STANDARD 655.25 FOR SIDEWALK RAMP AND DETECTABLE WARNING SURFACE DETAILS.
2. RAMP AND LANDING WIDTHS SHALL MATCH SIDEWALK WIDTH/MULTIUSE PATH WIDTH, 5' MIN.
3. CROSSWALK WIDTH VARIES BASED ON INTERSECTION CONDITIONS:
  - UNSIGNALIZED: 8' MIN.
  - MULTI-USE PATH: 12' MIN.
 OR AS DESIGNATED BY COUNTY ENGINEER BASED UPON EXPECTED CROSSWALK USAGE.
4. OPTIONAL BACK CURB AS NEEDED, DEPENDENT ON ADJACENT GRADING.
5. DESIGN CURB RAMPS IN CONJUNCTION WITH CROSSWALK CRITERIA. POSITION OF CURB RAMPS SHALL ALIGN WITH CENTER OF ADJACENT CROSSWALK. IF THERE IS NO CROSSWALK, STOP BAR SHOULD BE PLACED 6' FROM CENTER OF CURB RAMP.
6. ROW TO BE OFFSET 1' AND CHAMFERED FROM THE POINT WHERE THE LOW POINT OF THE RAMP (ON THE OUTSIDE) MEETS THE LANDING PAD. ROW OFFSET MAY BE ADJUSTED FOR MULTIUSE PATH.
7. SIDEWALK OR SIDEWALK WITH SEPARATED ONE-WAY BIKE LANE CAN BE USED, SEE LATEST PRINCE GEORGE'S COUNTY URBAN STREET DESIGN STANDARDS FOR MIXED-USE BOULEVARD CROSS SECTIONS.
8. AT THE DISCRETION OF THE COUNTY ENGINEER, CROSSWALKS SHALL BE INSTALLED PERPENDICULAR TO CURB LINE.
9. DETECTABLE WARNING SURFACE SHALL BE A CONTRASTING COLOR TO SIDEWALK. (YELLOW OR RED ONLY) SEE 655.25 FOR DETECTABLE WARNING SURFACE DETAILS.
10. AT THE DISCRETION OF THE COUNTY ENGINEER, STOP BAR SHOULD COMPLY WITH LATEST AASHTO GUIDELINES. SEE TABLE 3-1 AND 3-2 FOR INTERSECTION SIGHT DISTANCE REQUIREMENTS.

APPROVED

*Michael D. Johnson*

FOR MICHAEL D. JOHNSON, PE, DIRECTOR

12/09/24

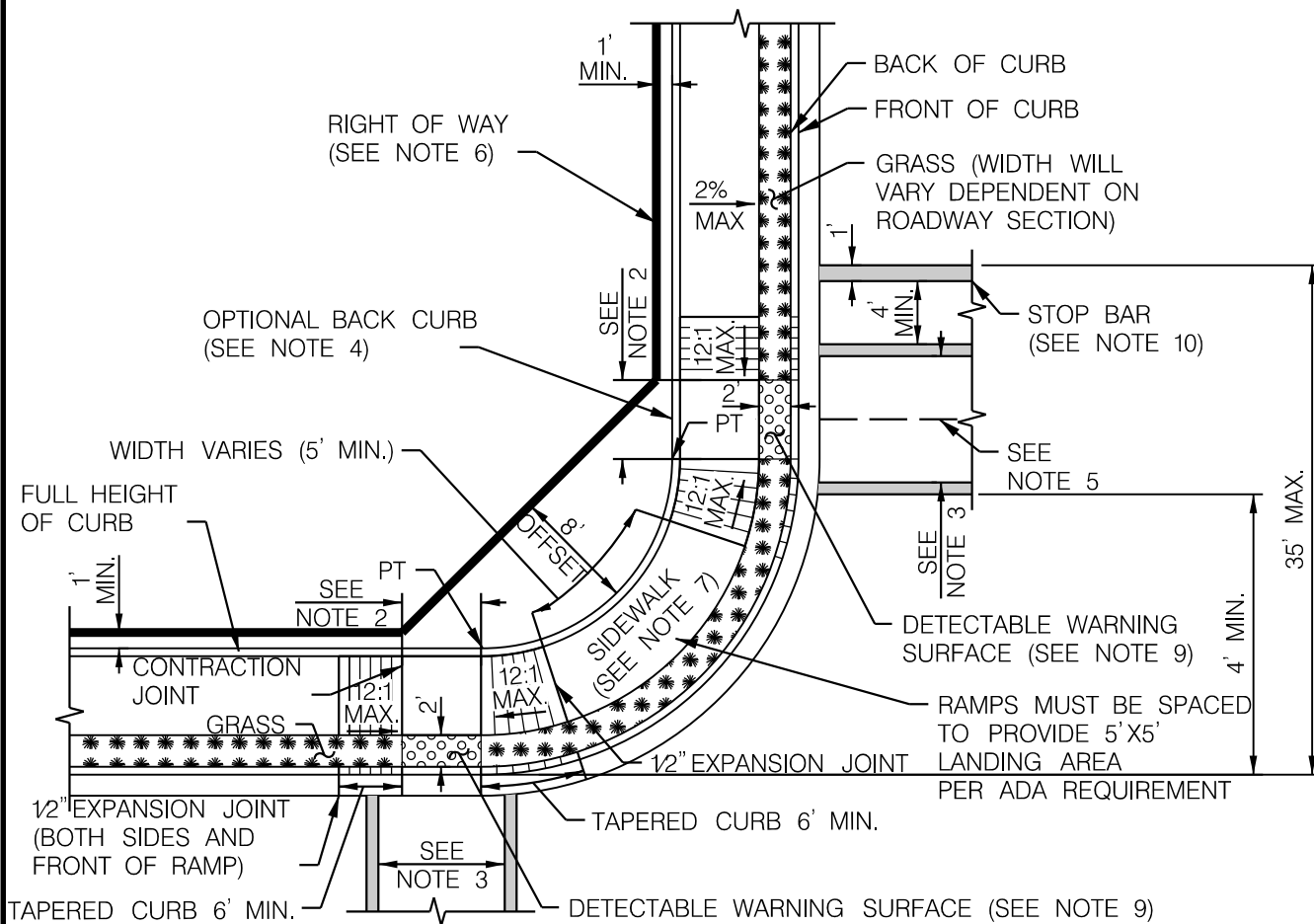
DATE



**DEPARTMENT OF PUBLIC WORKS  
AND TRANSPORTATION  
Prince George's County, MD**

**NON-SIGNALIZED  
INTERSECTION IMPROVEMENT DETAIL:  
COMBINATION/PARALLEL SIDEWALK  
RAMPS FOR 15-FOOT TO  
20-FOOT CURB RADIUS**

**STD.  
655.15**



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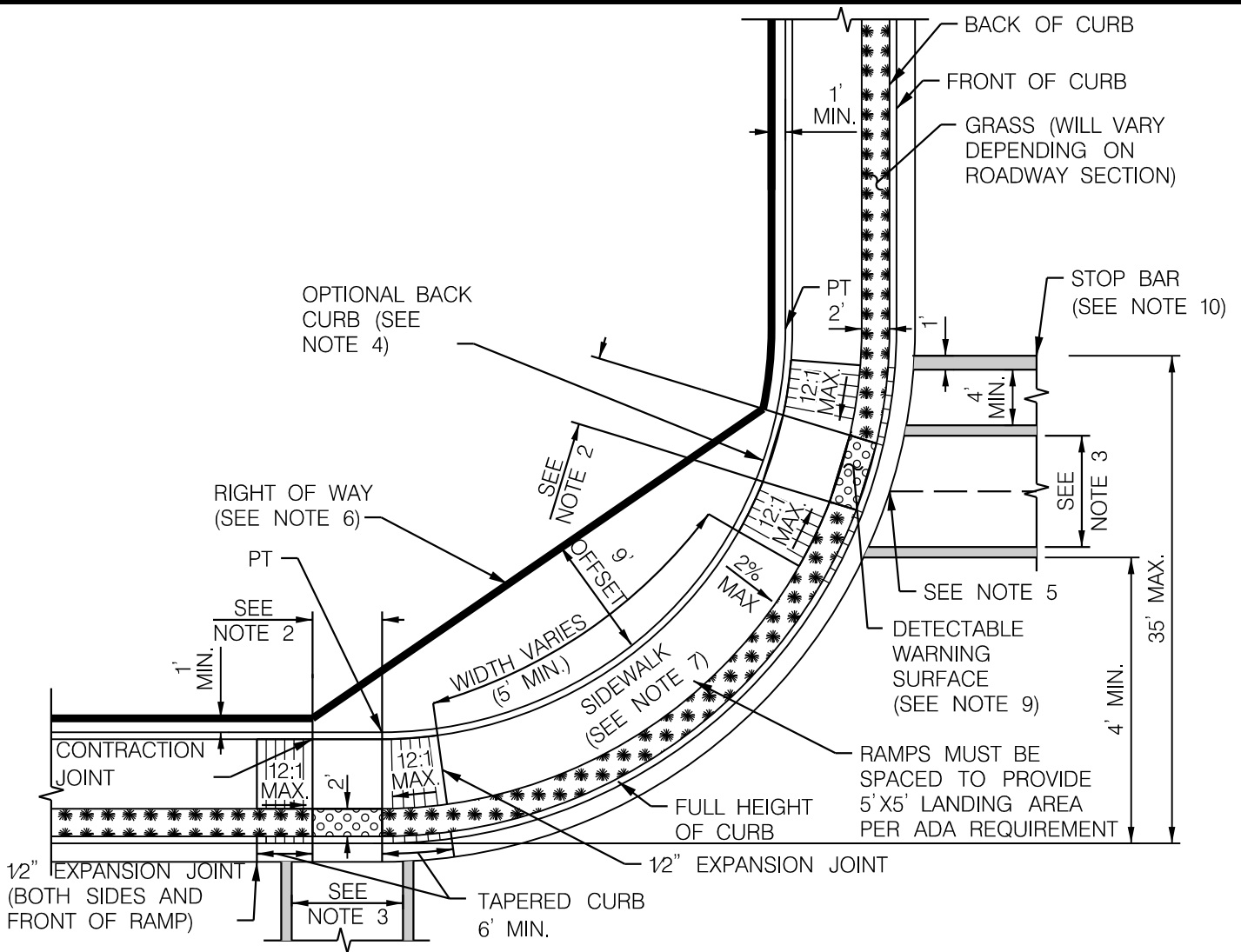
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**NON-SIGNALIZED  
INTERSECTION IMPROVEMENT DETAIL:  
COMBINATION/PARALLEL SIDEWALK  
RAMPS FOR 20-FOOT TO  
35-FOOT CURB RADIUS**

**STD.  
655.16**



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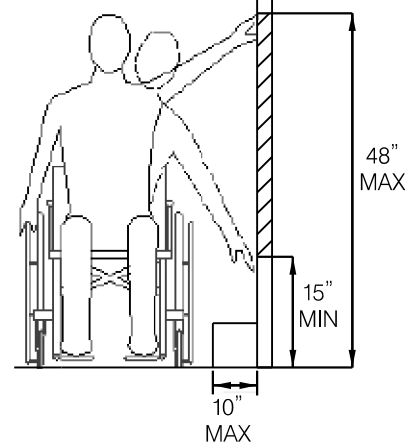
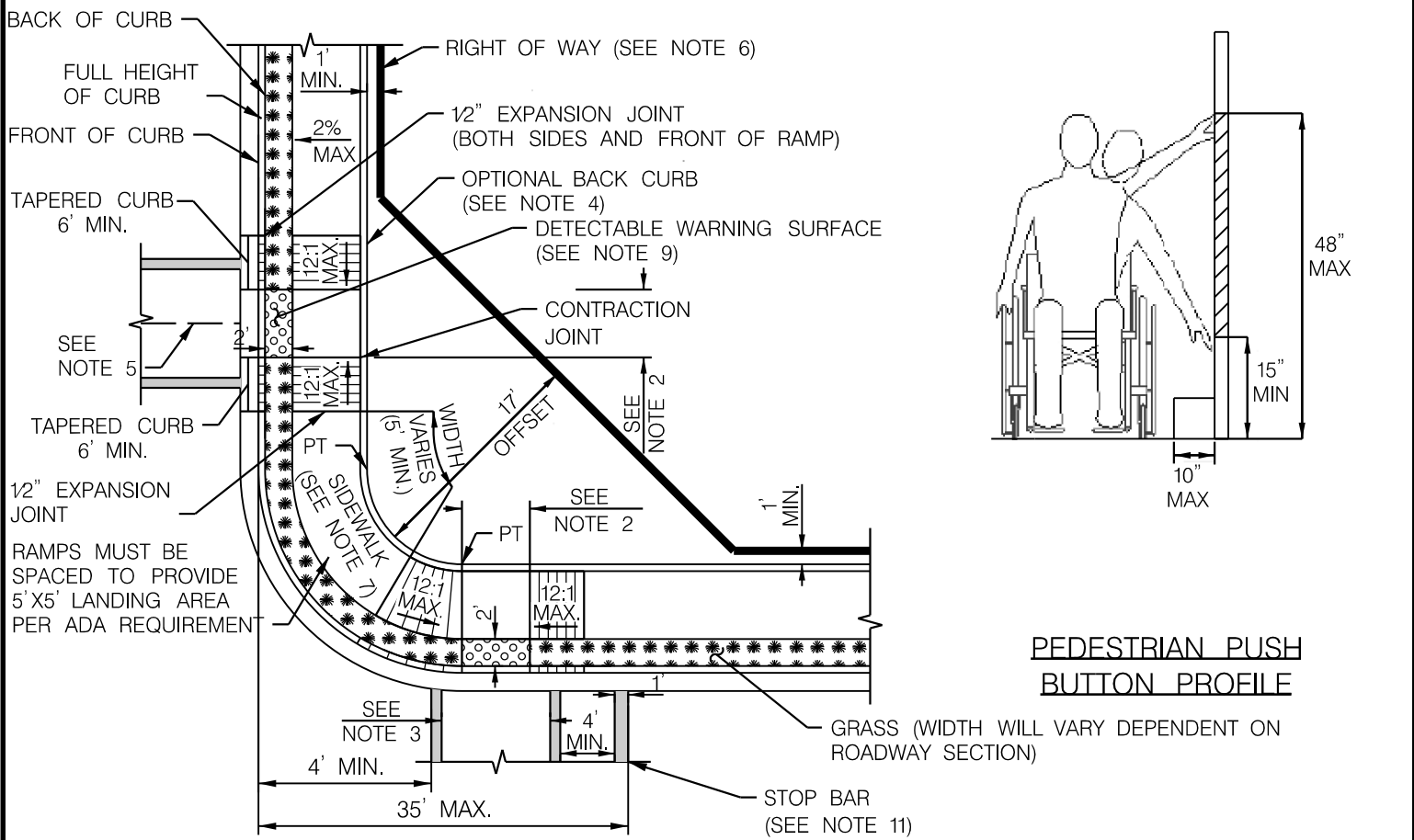


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Prince George's County, MD**

**NON-SIGNALIZED  
INTERSECTION IMPROVEMENT DETAIL:  
COMBINATION/PARALLEL SIDEWALK  
RAMPS FOR >35-FOOT CURB RADIUS**

**STD.**

**655.17**



**PEDESTRIAN PUSH  
BUTTON PROFILE**

**NOTES:**

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2. RAMP AND LANDING WIDTHS SHALL MATCH SIDEWALK WIDTH/MULTIUSE PATH WIDTH, 5' MIN.
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 OR AS DESIGNATED BY COUNTY ENGINEER BASED UPON EXPECTED CROSSWALK USAGE.
4. OPTIONAL BACK CURB AS NEEDED, DEPENDENT ON ADJACENT GRADING.
5. DESIGN CURB RAMPS IN CONJUNCTION WITH CROSSWALK CRITERIA. POSITION OF CURB RAMPS SHALL ALIGN WITH CENTER OF ADJACENT CROSSWALK.
6. ROW TO BE OFFSET 1' AND CHAMFERED 20' FROM CURB PT. ROW OFFSET MAY BE ADJUSTED FOR MULTIUSE PATH.
7. SIDEWALK OR SIDEWALK WITH SEPARATED ONE-WAY BIKE LANE CAN BE USED, SEE LATEST PRINCE GEORGE'S COUNTY URBAN STREET DESIGN STANDARDS FOR MIXED-USE BOULEVARD CROSS SECTIONS.
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9. DETECTABLE WARNING SURFACE SHALL BE A CONTRASTING COLOR TO SIDEWALK. (YELLOW OR RED ONLY) SEE 655.25 FOR DETECTABLE WARNING SURFACE DETAILS.
10. PEDESTRIAN PUSH BUTTONS SHALL BE PLACED AT A MAXIMUM 5' FROM EDGE OF CROSS WALK AND 6' FROM EDGE OF CURB. THE BUTTON SHALL BE NO LESS THAN 15" AND NO MORE THAN 48" FROM THE TOP OF SIDEWALK. THE SIDE REACH TO THE PUSH BUTTON SHALL NOT EXCEED 10".
11. AT THE DISCRETION OF THE COUNTY ENGINEER, STOP BAR SHOULD COMPLY WITH LATEST MUTCD GUIDELINES. SEE TABLE 4D-2 FOR MINIMUM SIGHT DISTANCE.

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FOR MICHAEL D. JOHNSON, PE, DIRECTOR

12/09/24

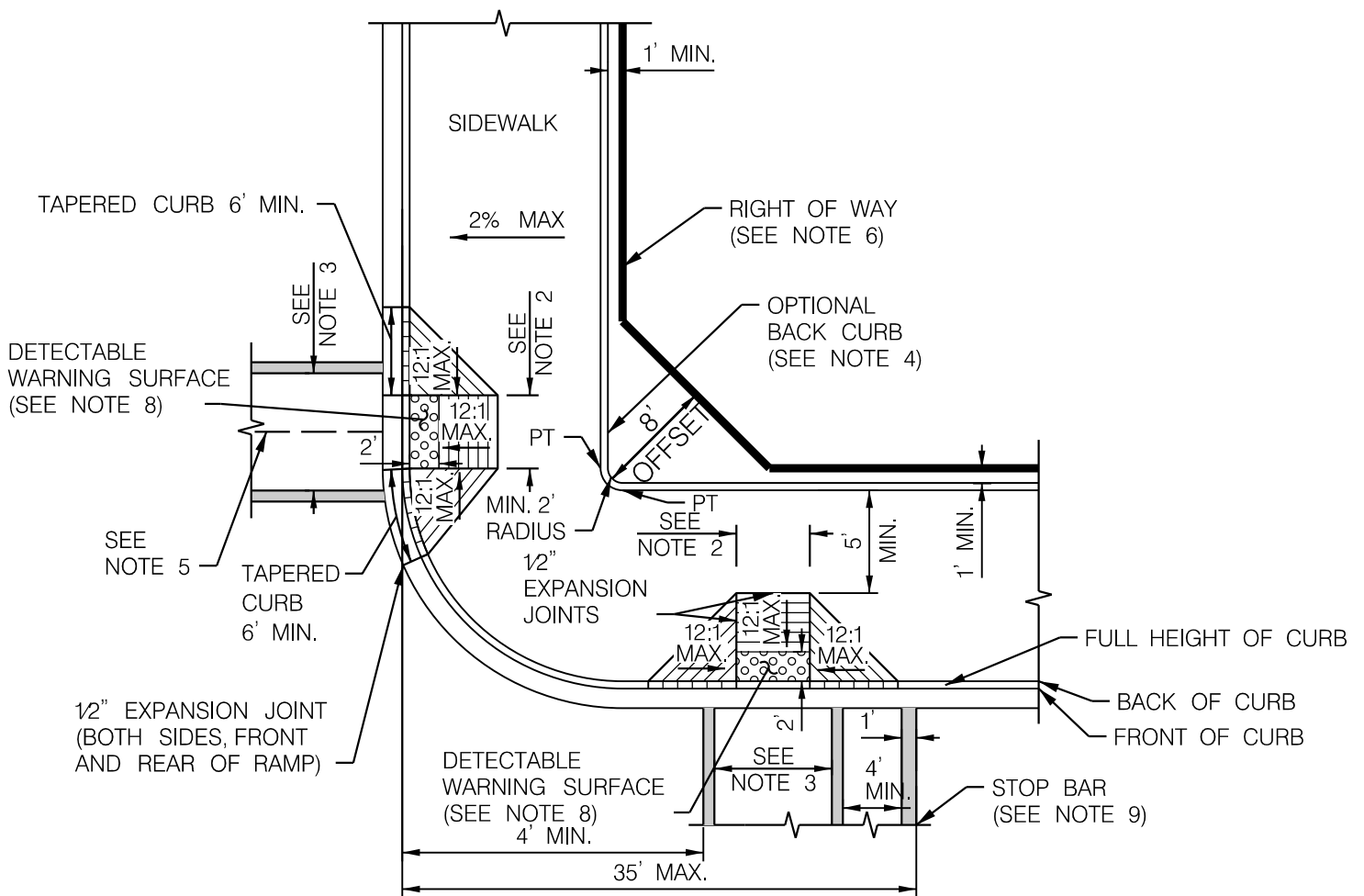
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Prince George's County, MD**

**SIGNALIZED  
INTERSECTION IMPROVEMENT DETAIL:  
COMBINATION/PARALLEL SIDEWALK  
RAMPS FOR 15-FOOT TO  
20-FOOT CURB RADIUS**

**STD.  
655.18**



**NOTES:**

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FOR MICHAEL D. JOHNSON, PE, DIRECTOR

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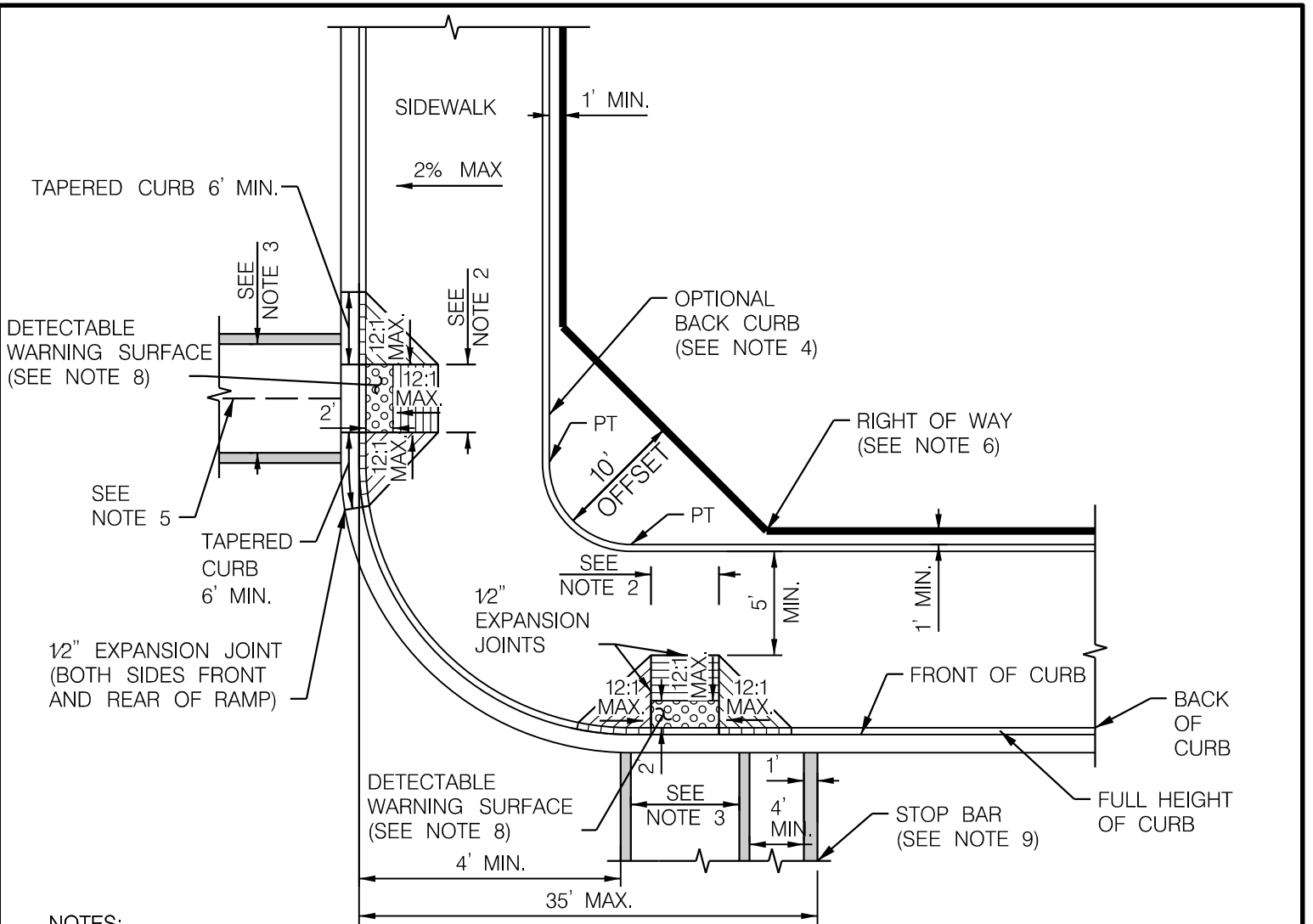


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Prince George's County, MD**

**NON-SIGNALIZED  
INTERSECTION IMPROVEMENT DETAIL:  
PERPENDICULAR SIDEWALK  
RAMPS FOR 15-FOOT TO  
20-FOOT CURB RADIUS**

**STD.  
655.19**





**NOTES:**

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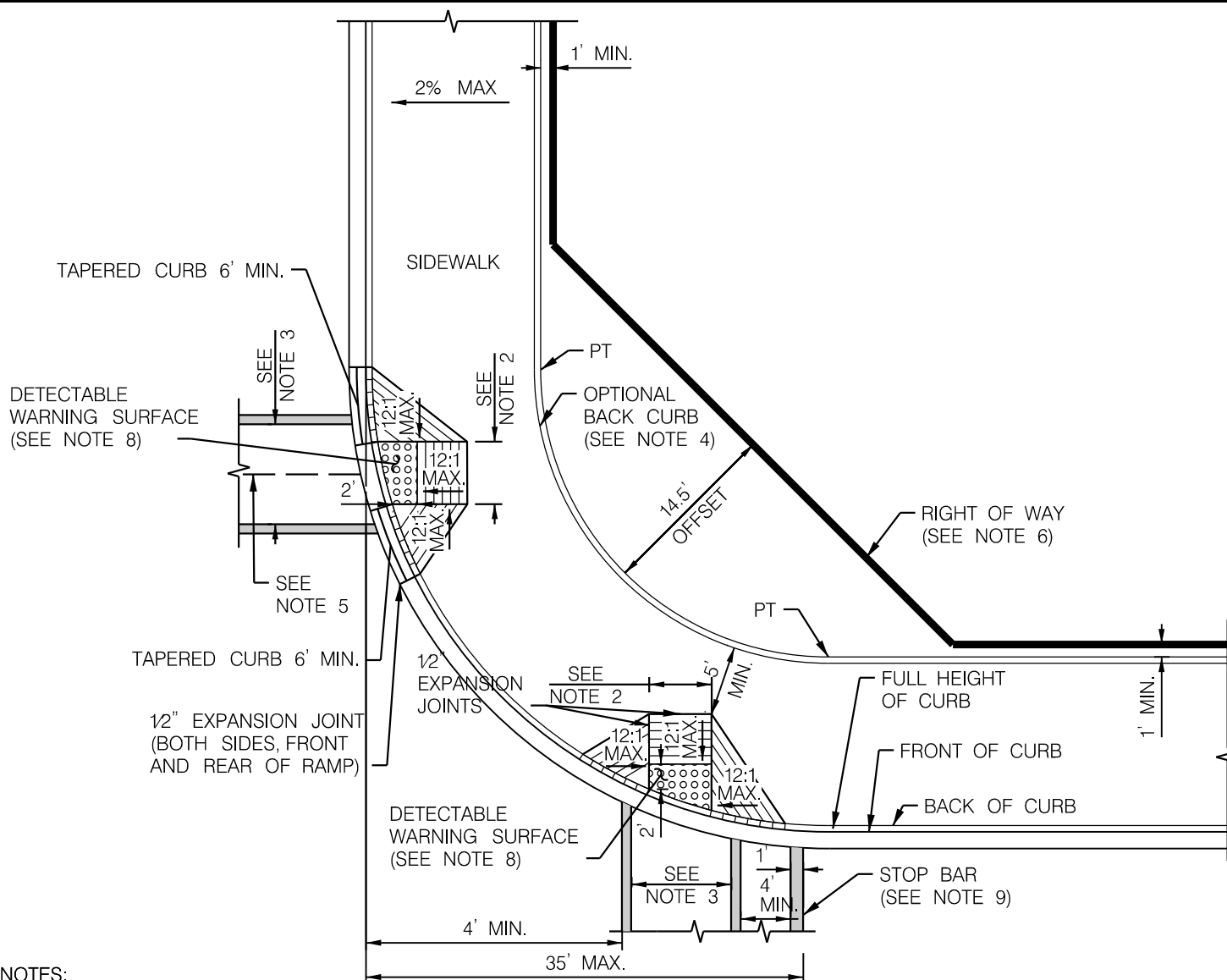
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Prince George's County, MD**

**NON-SIGNALIZED  
INTERSECTION IMPROVEMENT DETAIL:  
PERPENDICULAR SIDEWALK  
RAMPS FOR 20-FOOT TO  
35-FOOT CURB RADIUS**

**STD.  
655.20**



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DATE

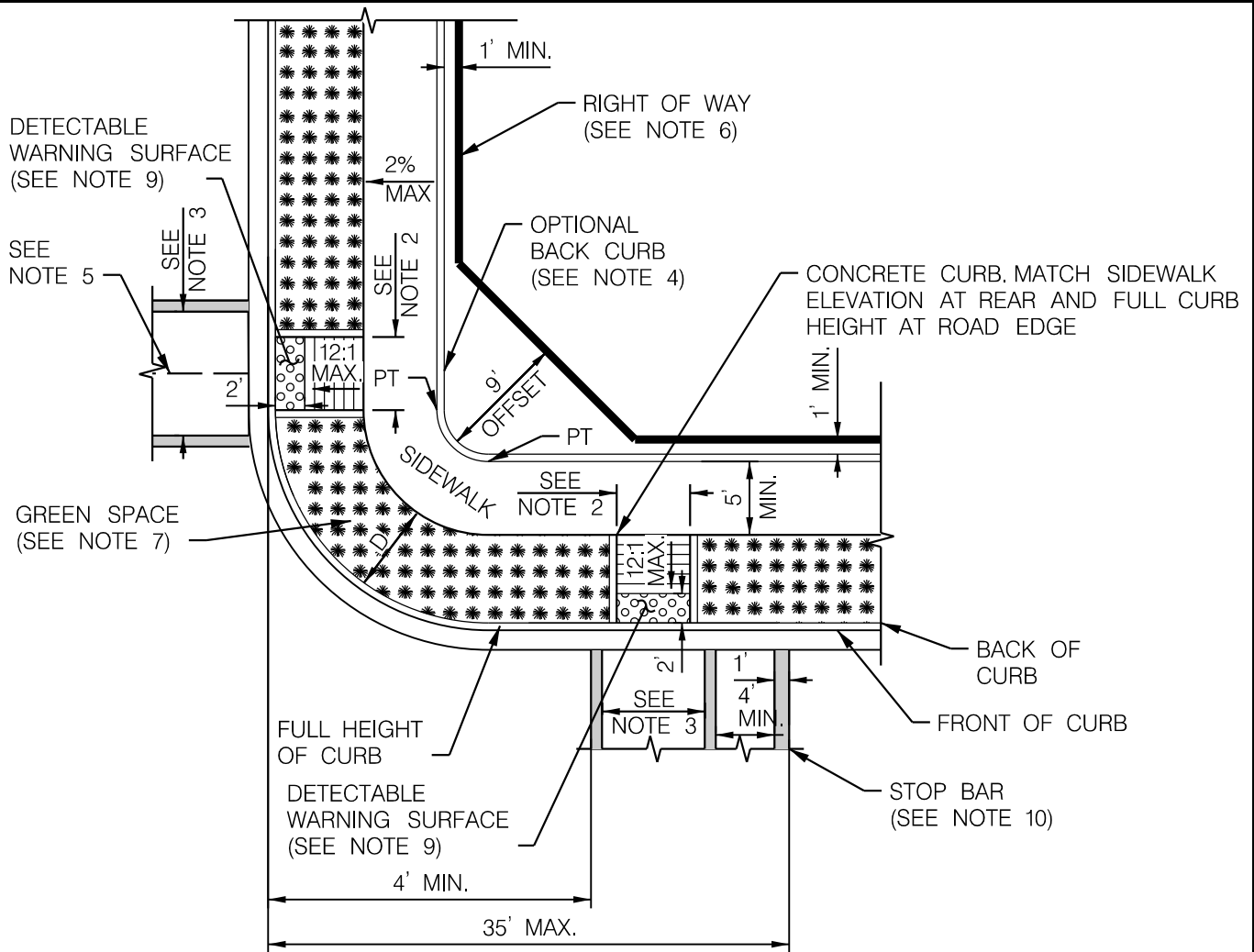


**DEPARTMENT OF PUBLIC WORKS  
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Prince George's County, MD**

**NON-SIGNALIZED  
INTERSECTION IMPROVEMENT DETAIL:  
PERPENDICULAR SIDEWALK RAMPS  
FOR >35-FOOT CURB RADIUS**

**STD.  
655.21**





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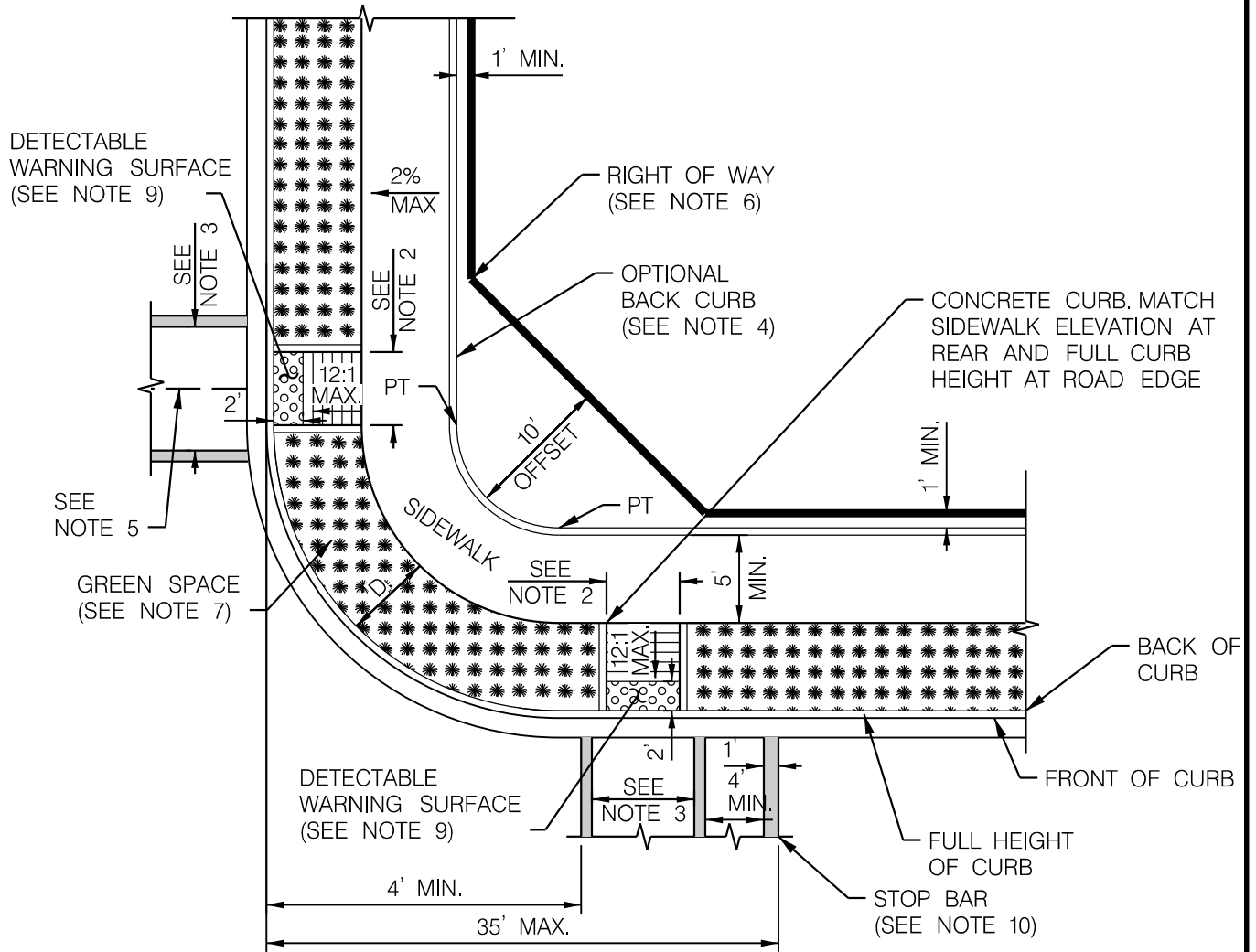
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**NON-SIGNALIZED  
INTERSECTION IMPROVEMENT DETAIL:  
PERPENDICULAR SIDEWALK WITH GREEN  
SPACE RAMPS FOR 15-FOOT TO  
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**STD.  
655.22**



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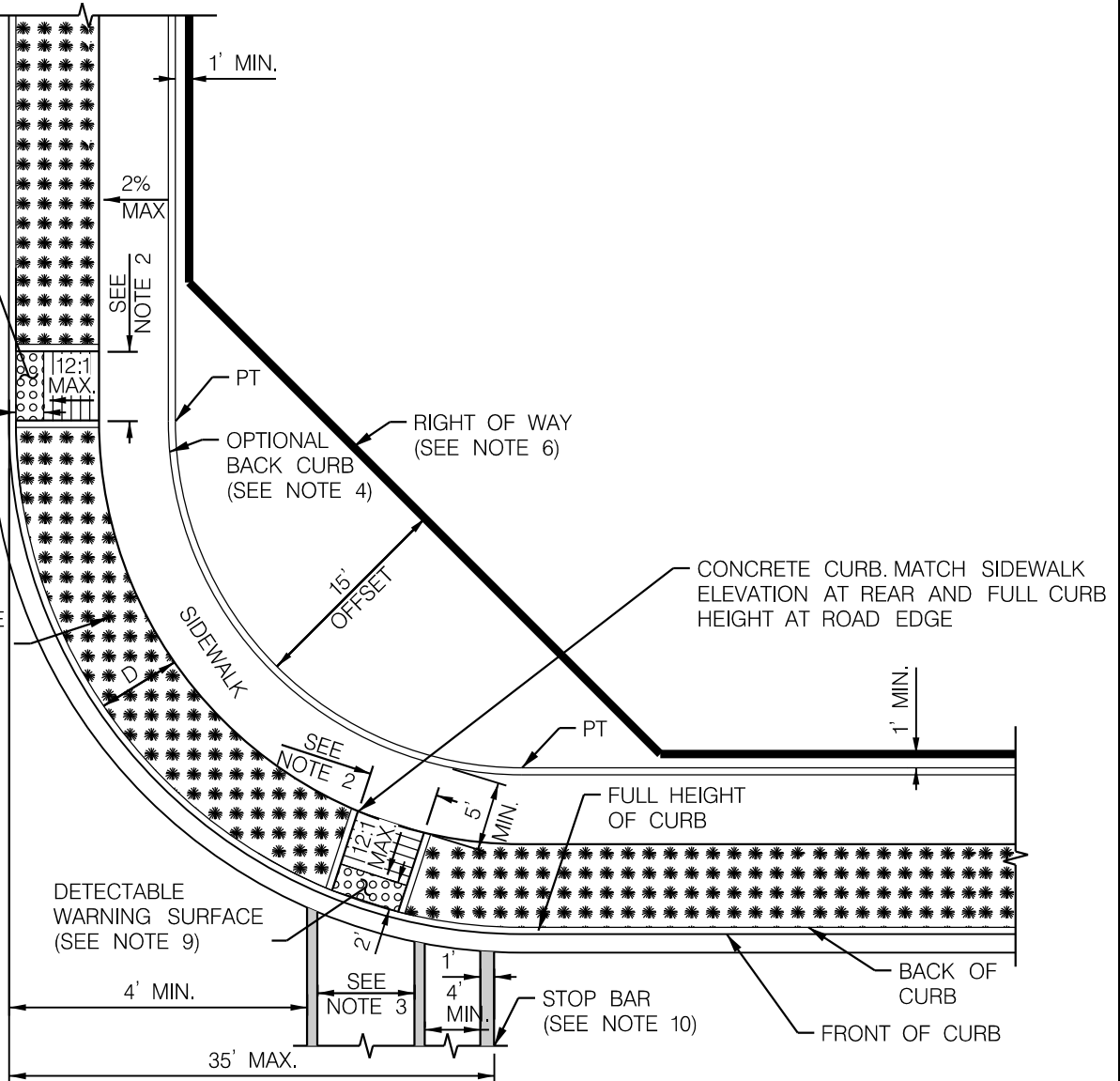
**STD.  
655.23**

DETECTABLE WARNING SURFACE (SEE NOTE 9)

SEE NOTE 3

SEE NOTE 5

GREEN SPACE (SEE NOTE 7)



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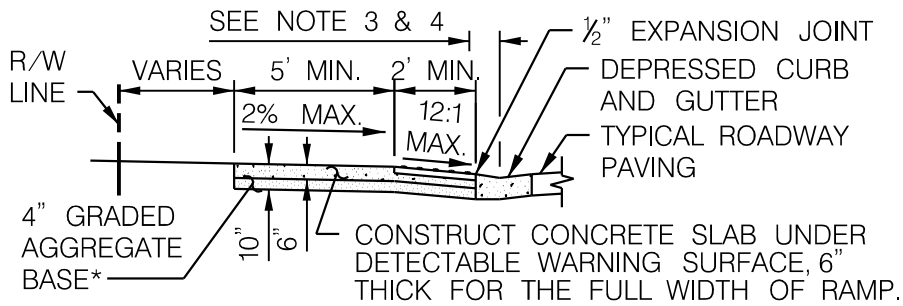
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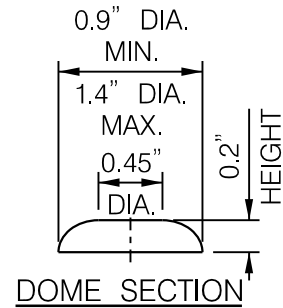
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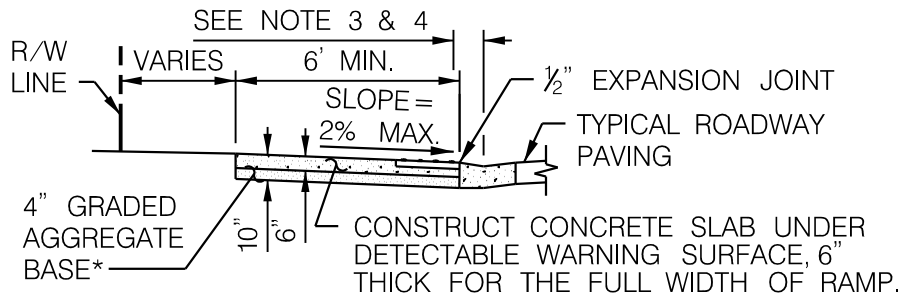
**STD.  
655.24**



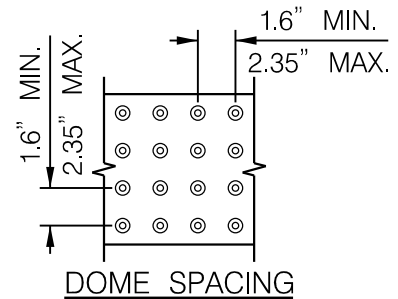
**CROSS SECTION – COMBINATION SIDEWALK RAMP**



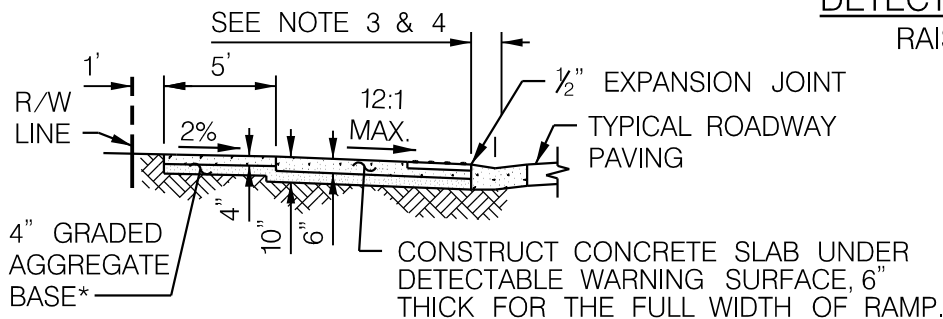
**DOME SECTION**



**CROSS SECTION – PARALLEL SIDEWALK RAMP**



**DOME SPACING**



**CROSS SECTION – PERPENDICULAR SIDEWALK RAMP**

**DETECTABLE WARNING SURFACE  
RAISED TRUNCATED DOMES**

\* NOTE: ALL SUB-GRADE BELOW THE GRADED AGGREGATE BASE SHALL BE PROPERLY COMPACTED

**GENERAL NOTES:**

1. ALL SIDEWALK RAMPS AND THEIR APPROACHES SHALL BE DESIGNED AND CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE ON WALKING SURFACES.
2. ALL CONCRETE SHALL BE MIX NO. 13 (4,000 PSI) AIR-ENTRAINED UNLESS OTHERWISE APPROVED BY THE DPW&T. THE SURFACE SHALL BE BROOM FINISHED.
3. DETECTABLE WARNING SURFACE SHALL BE INSTALLED DIRECTLY BEHIND THE CURB LINE AND EXTEND TWO FEET IN THE DIRECTION OF TRAVEL OVER THE FULL WIDTH OF THE RAMP, AND SHALL BE EMBEDDED SO AS TO BE FLUSH WITH THE SURROUNDING SURFACE.
4. FOR SKEWED APPLICATIONS THE DETECTABLE WARNING SURFACE SHALL BE CUT TO FIT THE BACK OF CURB ALIGNMENT WHILE STILL MAINTAINING THE MINIMUM OF TWO FEET IN THE DIRECTION OF TRAVEL.

APPROVED

*Michael D. Johnson*

FOR MICHAEL D. JOHNSON, PE, DIRECTOR

12/09/24  
DATE



**DEPARTMENT OF PUBLIC WORKS  
AND TRANSPORTATION  
Prince George's County, MD**

**NON-SIGNALIZED  
INTERSECTION IMPROVEMENT DETAIL:  
SIDEWALK RAMP AND  
DETECTABLE WARNING  
SURFACE DETAILS**

**STD.  
655.25**